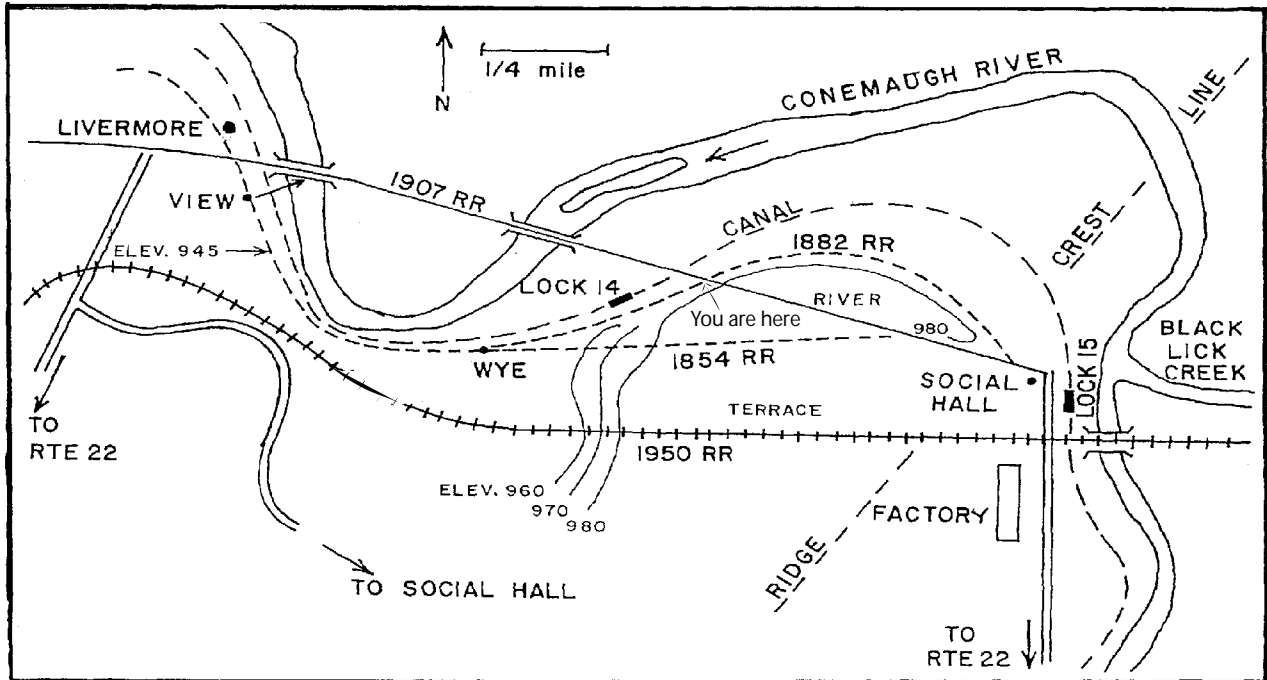


WEST PENN TRAIL

at
1882 – 1854 WYE



At a location 2,600 feet southwest of this sign post, the new bed of the West Penn Railroad, built in 1882, curves away from the original (1854) straight line of the railroad. Relocation of the tracks onto the new line was made so that trains would no longer have to climb the steep grade on the old line between the river bend and the level of the railroad bridge (970 feet) at Social Hall. The railroad south from Livermore was at 945 feet in elevation; less than a quarter of a mile east of this sign post, the elevation of the terrain is 980 feet (on the old river terrace).

A cut made across that terrace lowered the track bed to the 970-foot level, but that left a 25-foot difference in elevation between Livermore and the beginning of the terrace cut, less than a quarter of a mile east of this sign post - a climb of 25 feet in a little less than one mile (0.53% gradient). That steep ascent was avoided by curving the track around the edge of the terrace; that move lengthened the climb to the bridge to 1.75 miles, resulting in a slope (on a long ramp from Livermore) that was only 0.26% (2.6 feet climb per 1000 feet distance). Two stone culverts pass under the two roadbeds at this junction.